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Doc#: 0536218073 Fee: \$80.50
Eugene "Gene" Moore RHSP Fee: \$10.00
Cook County Recorder of Deeds
Date: 12/28/2005 04:27 PM Pg: 1 of 29

THIS DOCUMENT PREPARED BY
AND AFTER RECORDING
RETURN TO:

Alan M. Bell
Charity & Associates, P.C.
20 North Clark Street, Suite 1150
Chicago, Illinois 60602

(This space reserved for Recorder's Office)

MEMORANDUM OF BRIDGE PERMIT

THIS MEMORANDUM OF BRIDGE PERMIT (this "Memorandum") dated as of December 28, 2005, by the CITY OF CHICAGO, a municipal corporation and home rule unit of government under Article VII, Section 1 and Section 6(a), respectively, of the 1970 Constitution of the State of Illinois, having its principal office at 33 North LaSalle Street, Suite 600, Chicago, Illinois 60602 (the "City").

RECITALS:

WHEREAS, the U.S. Department of Transportation, United States Coast Guard ("DOT") granted the City that certain Bridge Permit attached hereto as Exhibit A (the "Bridge Permit"), which grants the City the right to construct, maintain and operate a rapid transit bridge facility to be used for mass transit purposes ("Orange Line Bridge Facility");

WHEREAS, the City accepted the terms and conditions of the Bridge Permit (as described on Exhibit B attached hereto);

WHEREAS, the City completed the construction of the Orange Line Bridge Facility pursuant to the terms of the Bridge Permit (as described on Exhibit C attached hereto);

WHEREAS, the City desires to record evidence of the Bridge Permit and the completion of the Orange Line Bridge Facility in the land records for Cook County, Illinois, as legally described on Exhibit D attached hereto; and

WHEREAS, the parcel identification numbers of the parcels on each side of the Orange Line Bridge Facility are as follows:

17-29-308-002-0000; 17-29-309-013-000; 17-29-309-020-000; 17-29-309-037-000.

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IN WITNESS WHEREOF, the parties hereto have caused this Agreement to be duly executed by their respective officers thereunto duly authorized as of the day and year first above written.

CITY OF CHICAGO

By: Stephen C. Hughes
Name: Stephen C. Hughes
Title: Acting City Comptroller

Property of Cook County Clerk's Office

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All-Purpose Acknowledgement

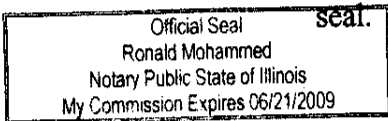
State of ILLINOIS)
County of COOK)

On 12-28-05 before me, Ronald Mohammed personally appeared STEPHEN RUGHES

personally known to me OR-

proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her their signature(s) on the instrument the person(s), or the entity upon behalf of which the persons acted, executed the instrument.

Witness my hand and official



Ronald Mohammed
SIGNATURE OF NOTARY

CAPACITY CLAIMED BY SIGNER

- INDIVIDUAL
- CORPORATE ACTING OFFICER(S) CITY COMPTROLLER TITLES(S)
- PARTNER(S)
- LIMITED
- GENERAL

- ATTORNEY-IN-FACT
- TRUSTEE(S)
- GUARDIAN/ CONSERVATOR

OTHER: _____

SIGNER IS REPRESENTING: NAME OF PERSON(S) OR ENTITY(IES)

CITY of CHICAGO

ATTENTION NOTARY: Although the information requested below is OPTIONAL, it could prevent fraudulent attachment of this certificate to an unauthorized document.

THIS CERTIFICATE MUST BE ATTACHED TO THE DOCUMENT DESCRIBED AT RIGHT:

Title or Type of Document _____

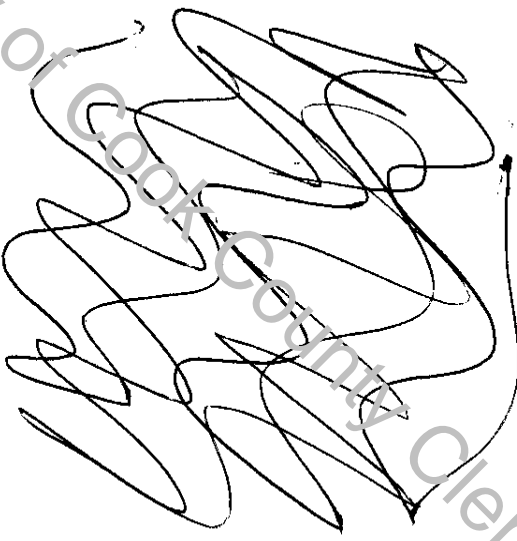
Number of Pages _____ Date of Document _____

Signer(s) Other than Named Above _____

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EXHIBIT A

Property of Cook County Clerk's Office



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Dec-28-05 13:38

From-Bureau of Highways

3127446438

T-657 P.002

F-871

M

U.S. Department
of Transportation

United States
Coast Guard



Commander,
Ninth Coast Guard District

1240 E. Ninth St.
Cleveland, Ohio 44199-2080
Staff Symbol: (687)
Phone: (216)522-3993

16591
SER#B-75/cdm
2 June 1987

DEPARTMENT OF PUBLIC WORKS
BUREAU OF TRANSPORTATION

JUN 11 1987

*cc: Kanya
Lynch
SWT permit
file*

Mr. Paul A. Karas
Commissioner of Public Works
Department of Public Works
320 North Clark Street, Room 411
Chicago, Illinois 60640

ACTION _____
FILE _____

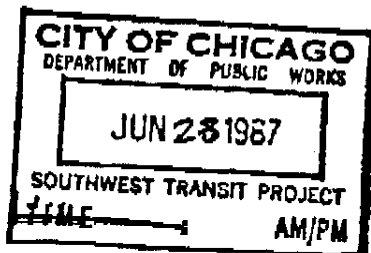
Dear Mr. Karas:

Your application of 12 November 1986 for Coast Guard approval of the plans of the Southwest Transit Bridge across the South Fork of the South Branch of the Chicago River, mile 5.77 at Chicago, Illinois has been approved by the Commandant, U. S. Coast Guard. The instrument of approval, Bridge Permit #(3-87-9), dated 20 May 1987, with the approved plans attached thereto, is enclosed. Strict compliance with all terms of the permit is required.

In accordance with Title 33 of the Code of Federal Regulations, Part 118, permanent bridge lighting required for the protection of navigation has been prescribed by authority of the District Commander. Coast Guard approval of the lighting is cited in enclosure two (2), and is forwarded for your records as your authority to display the lighting prescribed.

All navigational lights prescribed shall be displayed from sunset to sunrise and at other times when visibility is less than one mile; they shall be of sufficient candlepower to be visible against background lighting at a distance of at least 2,000 yards.

Lights should be installed as soon as construction work reaches the point where the new structure presents a hazard to navigation. Please notify this office when the lights have been placed into operation. Responsibility of the owner does not cease upon installation of the prescribed lights. A thorough program of inspection and maintenance is necessary to insure the navigation lighting is always properly displayed.



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T-657 P.009/020 F-871

This office shall be kept informed concerning the status of the project. This is to include the submission of monthly progress reports giving a brief description of work accomplished and showing the percentage of the project completed. Please advise us when the contract has been let for the construction of the bridge. The starting date of the construction is to be furnished to this office at least fifteen (15) days prior to commencement so we can apprise waterway users of the construction activities. The commencement notification shall address the following, as applicable:

1. Type of construction barges, by size, or other related equipment, to be utilized within the waterway.
2. Length of time the construction equipment will be in the waterway.
3. Statement that the floating equipment will be marked in accordance with the applicable "Rules of the Road."
4. Days of the week and work hours during which construction will be performed.
5. Information addressing floating operations, if such operations are to be utilized for construction of the subject bridge, or for removal of an existing structure.

Should it be determined by the contractor that construction activities will require restriction, either total or partial, to be placed upon navigational interests, commercial or recreational, the contractor shall contact this office no less than fifteen (15) days prior to the proposed restriction periods. The contractor shall provide information relative to the purpose of the restrictions, the periods of time the restrictions are to be imposed, and the duration of the restrictions. Additionally, should the contractor utilize a marine radio, or land telephone, for communications at the project site, the radio channels or telephone numbers being utilized shall be provided to this office. At no time during construction shall restrictions be placed upon navigation without first receiving approval from the Coast Guard. The Coast Guard will conduct periodic inspections to insure the work is not unreasonably interfering with navigation.

All correspondence directed to this office concerning the project shall state the name of the project, the name of the waterway over which the bridge is to be constructed, and the Coast Guard Bridge Permit Number.

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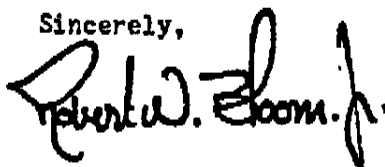
3127446438

T-657 P.004/020 F-871

Upon completion of the entire project, submit written certification (copies enclosed) to this office that the new structure has been built in accordance with the approved plans and that all conditions of the permit have been fulfilled. It is important that this certification be made since this information is published in various nautical publications which inform vessel masters and boaters of clearances for bridges, pipelines, and conveyors across navigable waters of the United States. Please provide two (2) 8" x 10" black and white photographs for our files, showing the completed structure as would be seen by the mariner, from both upstream and downstream.

Please acknowledge receipt of this permit.

Sincerely,



ROBERT W. BLOOM, JR.
Chief, Bridge Branch
By direction of Commander,
Ninth Coast Guard District

- Encl: (1) Bridge Permit #(3-87-9)
(2) Approved lighting permit
(3) Excerpt of Part 118 - Lighting of Bridges
(4) Recommendation for initial installation of lights
(5) Completion reports (two copies)

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T-657 P.005/020 F-871

U.S. Department
of Transportation
United States
Coast Guard

**BRIDGE PERMIT****20 MAY 1987**

(3-87-9)

WHEREAS by Title V of an act of Congress approved August 2, 1946, entitled "General Bridge Act of 1946," as amended (33 U.S.C. 525-533), the consent of Congress was granted for the construction, maintenance and operation of bridges and approaches thereto over the navigable waters of the United States;

AND WHEREAS the Secretary of Transportation has delegated the authority of Section 502(b) of that act to the Commandant, U. S. Coast Guard by Section 1.46(c) of Title 49 Code of Federal Regulations;

AND WHEREAS before construction is commenced, the Commandant must approve the location and plans of any such bridge and may impose any specific conditions relating to the construction, maintenance and operation of the structure which he deems necessary in the interest of public navigation, such conditions to have the force of law;

AND WHEREAS the - CITY OF CHICAGO - has submitted for approval the location and plans of a bridge to be constructed across the South Fork of the South Branch of the Chicago River at Chicago, Illinois;

NOW THEREFORE, This is to certify that the location and plans dated November 1986 are hereby approved by the Commandant, subject to the following conditions:

1. No deviation from the approved plans may be made either before or after completion of the structure unless the modification of said plans has previously been submitted to and received the approval of the Commandant.
2. The construction of falsework, cofferdams or other obstructions, if required, shall be in accordance with plans submitted to and approved by the Commander, Ninth Coast Guard District prior to construction of the bridge. All work shall be so conducted that the free navigation of the waterway is not unreasonably interfered with and the present navigable depths are not impaired. Timely notice of any and all events that may affect navigation shall be given to the District Commander during construction of the bridge. The channel or channels through the structure shall be promptly cleared of all obstructions placed therein or caused by the construction of the bridge to the satisfaction of the District Commander, when in the judgment of the District Commander the construction work has reached a point where such action should be taken, but in no case later than 90 days after the bridge has been opened to traffic.

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T-857 P.006/020 F-871

Continuation Sheet

Bridge across the South Fork of the South Branch
of the Chicago River at Chicago, Illinois

BRIDGE PERMIT

(3-87-9)

3. Issuance of this permit does not relieve the permittee of the obligation or responsibility for compliance with the provisions of any other law or regulation as may be under the jurisdiction of any federal, state or local authority having cognizance of any aspect of the location, construction or maintenance of said bridge.

4. When the proposed bridge is no longer used for transportation purposes, it shall be removed in its entirety or to an elevation deemed appropriate by the District Commander and the waterway cleared to the satisfaction of the District Commander. Such removal and clearance shall be completed by and at the expense of the owner of the bridge upon due notice from the District Commander.

6. The approval hereby granted shall cease and be null and void unless construction of the bridge is commenced within three years and completed within five years after the date of this permit.



J. M. SEABROOKE

Captain, U. S. Coast Guard

Chief, Bridge Administration Division

By direction of the Commandant

Cook County Clerk's Office

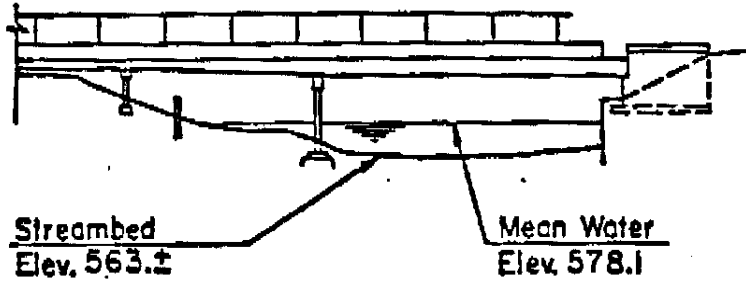
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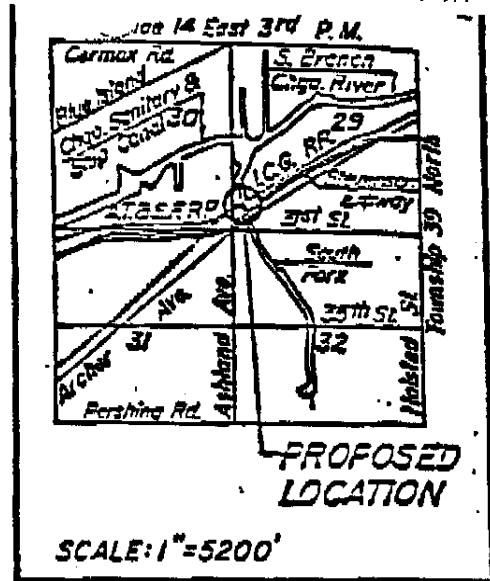


Streambed
Elev. 563.2

Mean Water
Elev. 578.1

ELEVATION
HORIZONTAL AND VERTICAL
SCALE: 1" = 80'

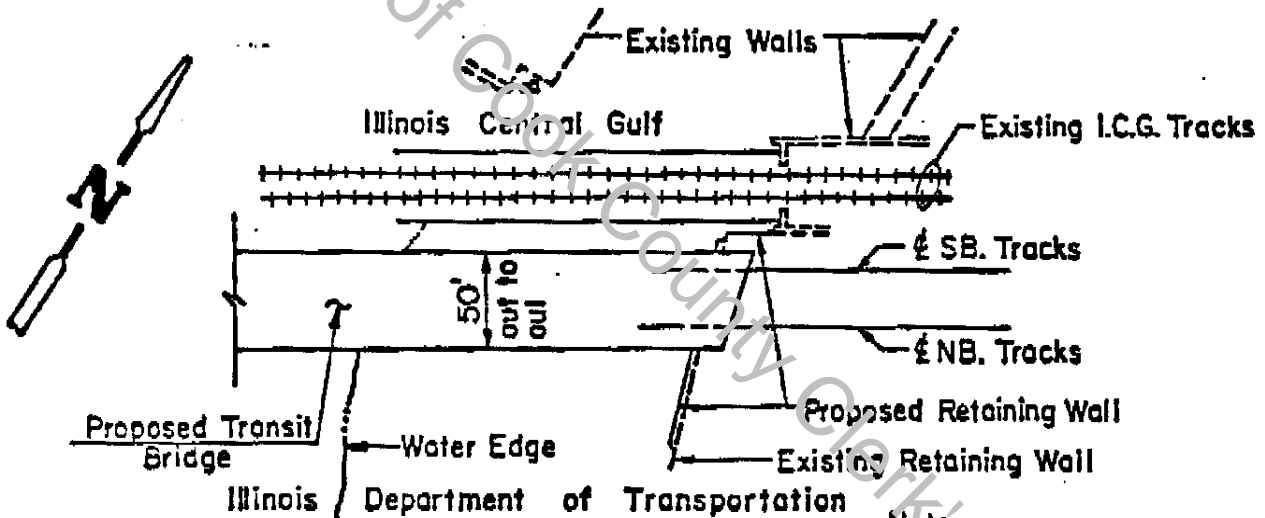
High Water Level Elev. 583.9
Low Water Level Elev. 575.9



**PROPOSED
LOCATION**

SCALE: 1" = 5200'

VICINITY MAP



PLAN

Note: See details on sheet 2.

NOTES:

- 1) Datum: Mean Tide New York 1935 Adj. at Chicago.
- 2) Disturbed areas will be sodded.



50 100 200
SCALE IN FEET

NO.	NAME	ADDRESS
1.	Illinois Department of Transportation	1000 Plaza Drive Schaumburg, Il. 60196
2.	Illinois Central Gulf	233 N. Michigan Ave. Chicago, Il. 60601

PROJECT DESCRIPTION:
SOUTHWEST TRANSIT
BRIDGE FROM ASHLAND AVE.
TO CHICAGO RIVER
LOCATION: Chicago, Illinois

NOV. 1986

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Dec-28-05

13:40

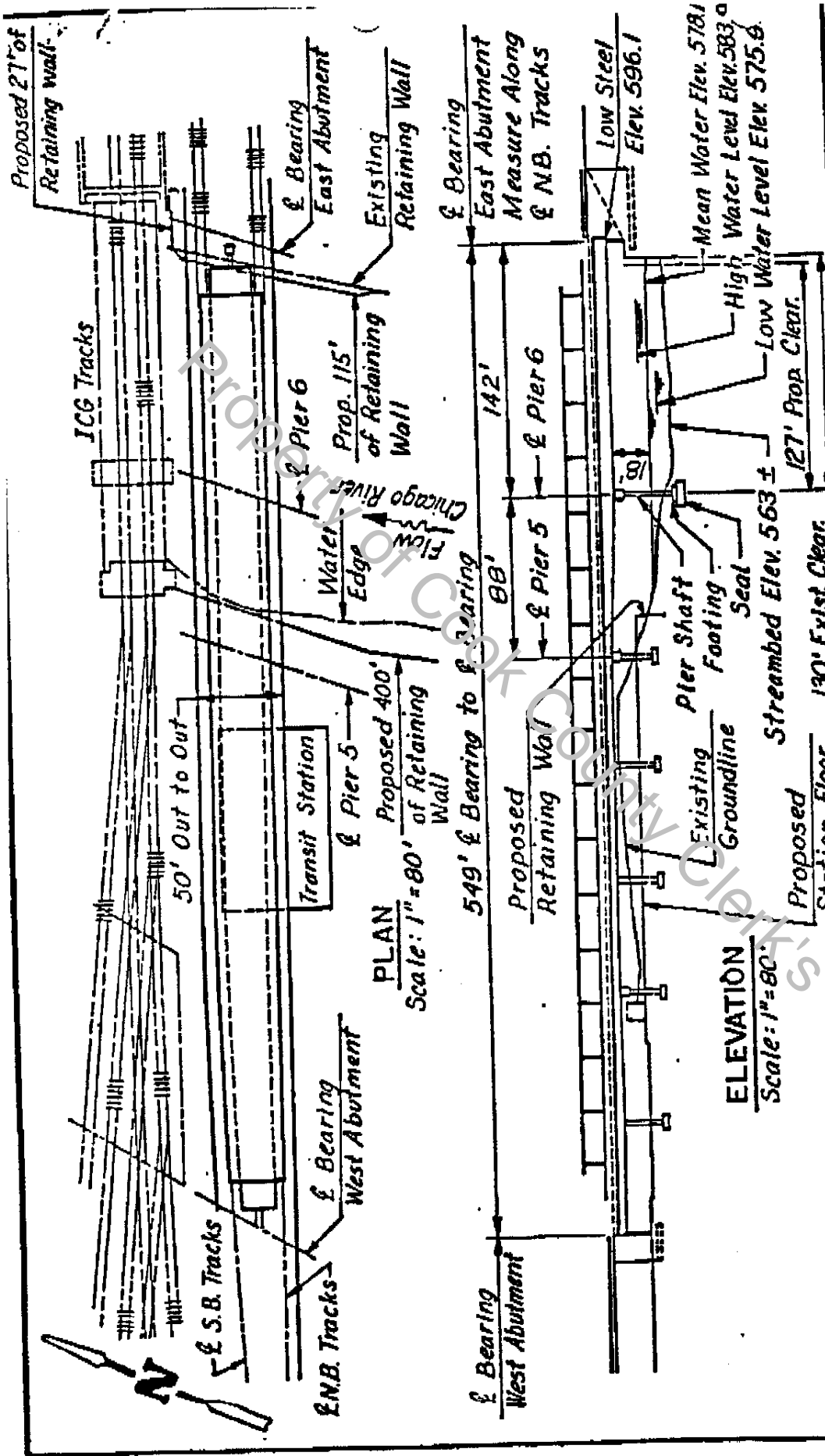
From-Bureau of Highways

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T-657

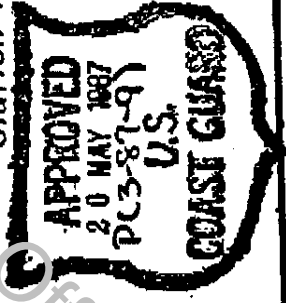
P. 008/020

F-871



PLAN
Scale: 1"=80'
549' Bearing to Bearing
142'
88'
18'
5'

ELEVATION
Scale: 1"=80'
130' Exlst. Clear.
127' Prop. Clear.
Low Steel Elev. 596.1
Mean Water Elev. 578.1
High Water Level Elev. 583.0
Low Water Level Elev. 575.6



DATUM: MEAN TIDE NEW YORK
1935 ADJ. AT CHICAGO.

SOUTHWEST TRANSIT BRIDGE
FROM ASHLAND AVENUE TO
CHICAGO RIVER
COOK COUNTY, ILLINOIS
NOV. 1986
CITY OF CHICAGO
DEPARTMENT OF PUBLICWORKS

Sheet 2 of 2

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Dec-28-05

13:40

From-Bureau of Highways

3127446498

T-857

P.009/020

F-871

Commander (obr)
Ninth Coast Guard District
1240 East Ninth Street
Cleveland, Ohio 44199
Telephone: (216) 522-3993

16595
2 June 1987

Mr. Paul A. Karas
Commissioner of Public Works
Department of Public Works
320 North Clark Street, Room 411
Chicago, Illinois 60610

Dear Mr. Karas:

Requirements for lighting of the Southwest Transit Bridge across the South Fork, South Branch of the Chicago River, mile 5.77 at Chicago, Illinois for the protection of navigation.

Bridge lighting is prescribed in accordance with Title 33, Code of Federal Regulations, Part 118.65, and is approved as indicated on the reverse hereof. Exact placement of the required lights on the bridge structure is explained in 33 CFR 118.65.

Sincerely,



ROBERT W. BLOOM, JR.
Chief, Bridge Branch
By direction of Commander,
Ninth Coast Guard District

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Dec-28-05 13:40

From-Bureau of Highways

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T-657 P.010/020 F-871

FEDERAL REGISTER, VOL. 40
NO. 113 - WEDNESDAY,
JUNE 11, 1975

PART 118 - LIGHTING OF BRIDGES

Sec.	
118.1	General requirements.
118.5	Penalty for failure to maintain.
118.10	Interference or obstruction prohibited.
118.15	Penalty for interference or obstruction.
118.20	Obtaining information.
118.25	Application procedure.
118.30	Action by Coast Guard.
118.35	Signals pertaining to the operation of bridges
118.40	Lighting during bridge construction.
118.45	Lighting for the protection of aerial navigation.
118.50	Inspection.
118.55	Periods of operation.
118.60	Visibility of lights.
118.65	Lights on fixed bridges.
118.70	Lights on swing bridges.
118.75	Lights on single-opening drawbridges.
118.80	Lights on bascule bridges.
118.85	Lights on vertical lift bridges.
118.90	Bridges crossing channel obliquely.
118.95	Lights on sheer booms.
118.100	Modification of requirements.
118.105	Bridges infrequently used and unusual cases.

AUTHORITY: Sec. 4, 34 Stat. 85, as amended, secs. 34, 85, 92, 633, 63 Stat. 500, 501, 503, 545; 33 U.S.C. 494, 14 U.S.C. 84, 85, 92, 633.

§ 118.1 General requirements.

All persons owning, occupying or operating bridges over the navigable waters of the United States shall maintain at their own expense such lights and other signals required for safety of maritime navigation as may be prescribed by the Commandant, and on any international bridge constructed after March 23, 1906, such additional signals as may be prescribed by the Commandant.

§ 118.5 Penalty for failure to maintain.

Any person required to maintain lights and other signals upon any bridge or abutment over or in the navigable waters of the United States who fails or refuses to maintain such lights and other signals, or to obey any of the lawful rules and regulations relating to the same is subject to a penalty as provided in 14 U.S.C. 85.

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T-657 P.011/020 F-871

S 118.10 Interference or obstruction prohibited.

No person shall obstruct or interfere with any lights or signals maintained in accordance with the regulations prescribed in this part.

S 118.15 Penalty for interference or obstruction.

Any person violating the provisions of S 62.01-10 of this chapter shall be deemed guilty of a misdemeanor and be subject to a fine not exceeding \$500 for each offense. Each day during which such violation shall continue shall be considered a new offense.

S 118.20 Obtaining information.

Persons desiring information concerning the marking of bridges shall address their inquiry to the District Commander having jurisdiction over the area concerned, or to the Commandant.

S 118.25 Application procedure.

Approval of lights and other signals required shall be obtained, prior to construction, from the District Commander of the area in which the structure will be situated. Application shall be by letter accompanied by duplicate sets of drawings showing (a) plan and elevation of the structure showing lights and signals proposed, and (b) small scale vicinity chart showing proposed bridge and all other bridges within 1,000 feet above or below the proposed bridge.

S 118.30 Action by Coast Guard.

(a) The District Commander receiving the application will review it and approve the lights and other signals proposed, or mark on the drawings, the lights and other signals required, and in the case of lights, cite the applicable section of this chapter which prescribes the lights required for the particular type bridge.

(b) Upon approval, one set of drawings will be returned to the applicant with the notation "navigational lights and/or other signals approved as shown" date, name and title of the District Commander.

S 118.35 Signals pertaining to the operation of bridges.

Lights and signals necessary for the execution of rules and regulations pertaining to the operation of bridges issued by the Secretary of the Army will be prescribed by the District Engineer.

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T-857 P.012/020 F-871

S 118.40 Lighting during bridge construction.

(a) While a bridge is under construction the District Commander having jurisdiction over the area in which the bridge is being built will prescribe the temporary lights and other signals to be displayed for the protection of navigation.

(b) When unusual conditions, exist, the District Commander will confer with the District Engineer having jurisdiction over the construction work before prescribing the temporary lights and other signals to be displayed during construction of the bridge.

(c) When the construction of a bridge is completed the permanent lights and other signals approved by the District Commander for the completed bridge shall be displayed.

S 118.45 Lighting for the protection of aerial navigation.

The owner of a bridge which constitutes a hazard to aerial navigation should maintain, in addition to the lights prescribed in this part, such lights as may be prescribed by the Administrator, Federal Aviation Agency.

S 118.50 Inspection.

Lights and signals maintained as required by the provisions of this chapter are subject to inspection at any time by the Coast Guard or such agent as may be appointed by the Commandant.

S 118.55 Periods of operation.

(a) Lights shall be displayed from sunset to sunrise and at other times when the visibility is less than one mile.

(b) Operators shall not be required to exhibit the prescribed lights during seasons when vessels are unable to navigate in the vicinity of the bridge.

(c) The operation of signals other than lights shall be as prescribed by the District Commander. Each case shall be considered individually.

S 118.60 Visibility of lights.

All lights required by the regulations in this part shall be securely attached to the structure and shall be of sufficient candlepower as to be visible against the background lighting at a distance of at least 2,000 yards 90 percent of the nights of the year. They shall be located as prescribed by the Commandant, with colors and arcs of visibility as specified.

(CCFR 58-50, 24 F.R. 5508, July 11, 1959)

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T-857 P.013/020 F-871

S 118.65 Lights on fixed bridges.

(a) Each fixed bridge span over a navigable channel shall be lighted so that the center of the navigable channel under each span will be marked by a range of two green lights, and each margin of each navigable channel will be marked by a red light: Provided, That when a margin of a channel is limited by a pier, only those lights prescribed in paragraph (b) of this section shall be required to mark such channel margin. The green lights shall each show through a horizontal arc of 180° ; they shall be securely mounted just below the outermost edge of the bridge span structure so as to be visible from an approaching vessel. Each red light shall show through a horizontal arc of 180° , and shall be securely mounted just below the outermost edge of the bridge span structure to show 90° on either side of a line parallel to the axis of the channel so as to be visible from an approaching vessel.

NOTE—Until such time that major repairs to or replacements of existing fixed span navigation lights colored green are made, it is permitted that only one of these lights marking the centerline of the same channel under a span shall be visible to an approaching vessel. When major repairs to or replacement of such existing green lights are made they shall conform with this paragraph.

(b) Pier lights. When the navigable channel extends from pier to pier or when piers are located within the navigable channel, each end of such piers shall be lighted with a red light. Each such light shall show through a horizontal arc of 130° , and shall be securely fastened at the end of the pier as low as practicable but not lower than 2 feet above navigable high water to show 90° on either side of a line parallel to the axis of the channel so as to be visible from an approaching vessel.

(c) Main channel. When necessary, the District Commander may prescribe that fixed bridges having two or more spans over a navigable channel shall have the main channel span marked with a set of three white lights arranged in a vertical line directly above each green light on the main channel span. Each white light shall show through a horizontal arc of 180° , and shall be mounted so that $\frac{1}{2}$ of the horizontal arc will show on either side of a line parallel to the axis of the channel. These three white lights shall be securely mounted on the bridge structure and spaced as nearly 15 feet apart as the structure of the bridge will permit, with a minimum spacing of 7 feet. The lowest white light in the line of three lights shall be placed not less than 10 nor more than 15 feet above each green light on the main channel span.

NOTE: Until such time that major repairs to or replacements of existing main channel lights showing white are made, it is permitted that these lights show through a horizontal arc of not less than 60° nor more than 180° with $\frac{1}{2}$ of such arc showing either side of a line parallel to the axis of the main channel. When major repairs or replacement of such existing white lights are made, they shall conform with this paragraph.

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13:42

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T-657

P. 014/020

F-871

§ 118.70 Lights on swing bridges.

(a) Swing span lights on through bridges. Each swing span of every through swing bridge shall be lighted with three lanterns so that when viewed from an approaching vessel the swing span when closed will display three red lights on top of the span structure, one at each end of the span on the same level and one at the center of the span no less than 10 feet above the other two lights, and when open for navigation will display three green lights on top of the span structure in a line parallel to and directly above the long axis of the span, one at each end of the span on the same level, and one at the center of the span no less than 10 feet above the other two lights. Each lantern shall show through alternate red and green horizontal arcs of 60° each, the axis of adjacent arcs to be 90° from each other; each light shall be securely mounted with the axis of the green arcs parallel to the long axis of the swing span.

(b) Swing span lights on deck and half-through bridges. Each swing span of every deck, half-through, girder, or similar type swing bridge shall be lighted with four lanterns so that when viewed from an approaching vessel the swing span when closed will display one red light at each end, and when open to navigation will display two green lights from each end. Each lantern shall show through one red and two green horizontal arcs of 60° each, the axis of each green arc to be 90° from the axis of the red arc; each light shall be securely mounted at the floor level of the span as near to the side of the span as practicable with the axis of the red light normal to the long axis of the swing span and so that the red light will be visible from an approaching vessel when the span is closed.

(c) Pier lights. Every swing bridge shall be lighted so that each end of the piers adjacent to the navigable channel (draw piers) or each end of their protection piers, (draw pier protection piers) and each end of the piers protecting the pivot pier (pivot protection pier) will be marked by a red light. Each of these lights shall show through a horizontal arc of 130° and shall be mounted as low as practicable below the floor level of the swing span to show 90° on either side of a line parallel to the axis of the channel so as to be visible from an approaching vessel.

(d) Axis lights. Every swing bridge shall be lighted so that the intersection of the bridge axis with each side of the pivot pier and the channel side of each draw pier which has a protection pier will be marked by a red light. Provided, That if the draw and draw protection piers are straight along their channel faces these lights shall not be required. Each such light shall show through a horizontal arc of 180° , and shall be mounted on the navigable channel face of the pier as low as practicable below the floor level of the swing span to show 90° either side of a line normal to the axis of the navigable channel so as to be visible from an approaching vessel.

(e) Omission of lights. Where the permanent navigable channel passes on only one side of the pivot pier of any swing span, the District Commander may authorize the omission of lighting of the unused channel.

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Dec-28-05 13:42

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3127446438

T-857 P.015/020 F-871

S 116.75 Lights on single-opening drawbridges.

(a) Bridges in this class. Bridges of the folding, pontoon and similar type single opening drawbridges are included in this class.

(b) Draw span lights. Each draw span of every single opening drawbridge shall be lighted with two lanterns so that when viewed from an approaching vessel the draw span when closed will display two red lights, one at each end of the span and when open to navigation will display two green lights, one at each end of the span. Each lantern shall show alternate red and green horizontal arcs of 60° each, the axis of adjacent arcs to be located 90° from each other; each lantern shall be securely mounted 15 feet above the roadway with the axis of the green arcs parallel to the long axis of the swing span.

(c) Pier or abutment lights. Every swing bridge shall be lighted so that the be mounted as described in paragraph end of each pier, abutment or fixed portion of the bridge adjacent to the navigable channel through the draw, or each end of the protection piers for such piers, abutments, or fixed portion of the bridge will be marked by a red light. Each red light shall show through an arc of 180°, and shall be securely mounted on the pier, abutment or fixed portion of the bridge as low as practicable to show 90° on either side of a line parallel to the axis of the channel so as to be visible from an approaching vessel.

S 118.80 Lights on bascule bridges.

(a) Lift span lights. Each lift span of every bascule bridge shall be lighted so that the free end of the span will be marked on each side by a green light which shows only when the span is fully open for the passage of a vessel and by a red light which shows for all other positions of the lift span. Each red and each green light shall show through a horizontal arc of 180°. The lighting apparatus shall be securely mounted to the side of the span so that the light will show equally on either side of a line parallel to the axis of the channels, so that they will be visible from an approaching vessel.

NOTE: Until such time that major repairs to or replacement of lift span navigation lights are made, existing lights may show through a horizontal arc of less than 180°. When major repairs to or replacement of existing lights are made they shall conform with this paragraph.

(b) Multiple parallel lift span lights. The outermost side of each outer span of every bascule bridge with parallel multiple lifts shall be lighted as prescribed in paragraph (a) of this section; the lights shall be controlled so that the green lights will be displayed only when all spans are open for navigation. The inner sides of the outer lift span and both sides of each inner

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lift span of every bascule bridge shall be lighted for all positions of the lift span. These lights shall be the same size of illumination and shall be mounted as described in paragraph (a) of this section.

(c) Pier lights. Every bascule bridge shall be lighted so that each end of every pier, or protection pier where provided, in or adjacent to the navigable channels under the lift span or spans will be marked by a red light. Each such red light shall show through a horizontal arc of 180° , and shall be securely mounted as low as practicable on the end of the pier, or protection pier, to show 90° either side of a line parallel to the axis of the navigable channel so as to be visible from an approaching vessel.

(d) Axis lights. Every bascule bridge which has at least one pier provided with a protection pier shall be lighted so that the intersection of the long axis of the lift span with the channel side of each pier, or protection pier, will be marked by a red light: Provided, That if all such piers and protection piers are straight along their channel faces these lights shall not be required. Each such red light shall show through a horizontal arc of 180° and shall be securely mounted on the navigable channel face of the pier as low as practicable to show 90° on either side of a line normal to the axis of the navigable channel so as to be visible from an approaching vessel.

S 118.65 Lights on vertical lift bridges.

(a) Lift span lights. The vertical lift span of every vertical lift bridge shall be lighted so that the center of the navigable channel under the span will be marked by a range of two green lights when the vertical lift span is open for navigation, and by one red light on each side of all other positions of the lift span. The green lights shall each show through a horizontal arc of 360° ; they shall be securely mounted just below the outermost edge of the bridge span structure so as to be visible from an approaching vessel. Each red light shall show through a horizontal arc of 180° , and shall be securely mounted just below the outermost edge of the lift span to show 90° on either side of the line parallel to the axis of the channel so that only one such light will be visible from an approaching vessel.

NOTE.—Until such time that major repairs to or replacement of lift span navigation lights are made, it is permitted that these lights show through a horizontal arc of not more than 60° . When major repairs to or replacement of such existing lights are made they shall conform with this paragraph.

(b) Pier lights. Every vertical lift bridge shall be lighted so that each end of every pier in or adjacent to navigable channels under the lift span, or each end of every protection pier when provided, will be marked by a red light. Each such light shall show through a horizontal arc of 180° , and shall be securely mounted as low as practicable on the end of the pier, or the pro-

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tection pier, to show 90° on either side of a line parallel to the axis of the navigable channel so as to be visible from an approaching vessel.

(c) Axis lights. Every lift bridge which has at least one pier provided with a protection pier shall be lighted so that the intersection of the lift span axis with the channel side of each pier adjacent to the navigable channel will be marked by a red light: Provided, That if every such pier, or protection pier, is straight along its channel face these lights shall not be required. Each such light shall show through a horizontal arc of 180° , and shall be securely mounted on the navigable channel face of the pier as low as practicable, to show 90° on either side of a line normal to the axis of the navigable channel so as to be visible from an approaching vessel.

S 118.90 Bridges crossing channel obliquely

Bridges crossing a body of water at an angle other than 90° with the axis of the channel shall be lighted in accordance with the regulations in this part with such modifications as are necessary in each particular case.

S 118.95 Lights on sheer booms.

The lights on sheer booms, isolated piers, and obstructions not part of the bridge or bridge approach structure come under the purview of S 66.01-35 of subchapter C of Title 33 of the Code of Federal Regulations and shall show a white or green light if kept on the left of vessels approaching from seaward, and shall show a white or red light if kept on the right of vessels approaching from seaward. For rivers the same rule shall apply, white or green lights shall be shown from the right ascending bank; white or red lights to be shown from the left descending bank. The color of the light and its characteristics (fixed, flashing, occulting, etc.) shall be determined by the District Commander.

S 118.100 Modification of requirements.

The District Commander may modify or change the requirements for the display of lights and signals on any bridge within the purview of the regulations in this part when a change in local conditions warrant such modification.

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§ 118.105 Bridges infrequently used and unusual cases.

Bridges over waterways with no significant nighttime navigation may be exempted from the lighting requirements of this part in whole or in part with the approval of the district commander. The district commander may modify the prescribed lights or require special lighting in specific or unusual cases where the lights prescribed in this part may not provide adequately for the safe passage of nighttime navigation.

(14 U.S.C. 633; 33 U.S.C. 499, 521, 49 U.S.C. 1655(g), 33 CFR 1.05-1(c)(4); 49 CFR 1.46 (b), 1.46(c)(3)(5))

Effective date. These amendments shall become effective July 11, 1975.

Dated: June 4, 1975

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RECOMMENDATIONS FOR INITIAL INSTALLATIONS**OF****BRIDGE NAVIGATION LIGHTS**

The following is furnished to assist in the selection of the proper equipment to maintain the marine navigation lights on bridges as required by Title 33, §118, Code of Federal Regulations. It is suggested that your installation incorporate the features outlined below:

a. That fresnel lens lanterns be used and installed in duplex arrangement, providing a service lamp and a standby lamp, each duplex standby lamp lights on burnout of the service lamp. Fresnel equipment embracing duplicate lenses in one lantern is available; lightout relays are also available.

b. That lamps for Fresnel equipment be 60 watt, 120 volt, A-21, clear, medium screw base, traffic signal lamp, or equivalent, all lamps to be properly focused in fresnel lenses.

c. That any equipment used for general illumination of the bridge be so designed that the light distribution pattern will not permit high intensity light to spill over and blind or interfere with marine navigation. There are a number of street light luminaries manufactured to control the light distribution by lens, ray collectors, hoods or shields. Proper consideration of this matter while the bridge design is in progress will avoid difficulties in the future.

It is the responsibility of the Permittee to maintain proper temporary navigational lighting and such other marking as may be prescribed on bridges during construction and permanent lighting on the bridge when completed.

The number and placement of the lights shall be approved by the Coast Guard prior to the commencement of the work.

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U. S. COAST GUARD (CG)
CG-159d

COMPLETION REPORT

CG-1113 (CG)

To:	NINTH COAST GUARD DISTRICT 1240 East Ninth Street Cleveland, Ohio 44199		INSTRUCTIONS		LOCATION. Indicate name of waterway, nearest town, route number if a highway bridge. TYPE OF BRIDGE. Abbreviate type of bridge: B-Beam, F-Fixed (except a suspension bridge), P-Posttension, TR-Trestle, VL-Vertical lift, Sus-Suspension, SW-Swing, RSP-Removable span. VERTICAL. Indicate the plane of reference used for measuring the vertical clearance, i.e. MSL, 2% line, elev. _____, etc. If additional space is required use remarks column. TYPE OF TRAFFIC. Abbreviate type of traffic: Hwy-Highway, Hwy-RR-Highway-railroad, RR-Railroad, FB-foot bridge. Indicate other types of traffic or use, such as pipelines, conveyor system, by plain language in remarks.		
Date superstructure commenced	Date superstructure completed	Date substructure commenced	Date substructure completed				
FOR CG USE ONLY	MILES ABOVE MOUTH	NAME & LOCATION	OWNER	TYPE OF BRIDGE	CLEARANCE	DATE PLANS APPROVED AND PERMIT NUMBER	TYPE OF TRAFFIC
					HORIZONTAL VERTICAL above low water datum for Lake Erie (IGLD-1955)		date opened to traffic etc:

The _____ Bridge was removed from the waterway in its entirety. All parts of the old bridge not incorporated in the new structure were removed to a depth of _____ feet below low water datum / to the natural bottom of the waterway. (If any parts of the old bridge are not incorporated in the new structure but retained in the waterway, include a copy of the Corps of Engineers permit or letter authorizing retention.)

I certify that the above bridge has been built in accordance with the approved plans (or in accordance with USCG approved modifications to the original permit) and that all conditions of the permit have been fulfilled.

Photographs of the completed structure as seen by the mariner (from upstream and downstream) are herewith submitted.

Additional remarks:


From: _____

SIGNATURE _____ DATE _____

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EXHIBIT B

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City of Chicago
Harold Washington, Mayor

Department of Public Works

Paul A. Karas
Commissioner

Richard G. Hankett
Deputy Commissioner

Bureau of Transportation
Planning and Programming
Room 411
320 North Clark Street
Chicago, Illinois 60610
(312) 744-7767

June 22, 1987

Mr. Robert W. Bloom, Jr.
Chief, Bridge Branch
United States Coast Guard
Ninth Coast Guard District
1240 East Ninth Street
Cleveland, Ohio 44199-2060

Re: Bridge Permit #(3-87-9)

Dear Mr. Bloom:

This will acknowledge receipt of the subject permit for the Southwest Transit Project bridge to be built over the South Fork of the South Branch of the Chicago River.

Very truly yours,

for Paul A. Karas
Commissioner of Public Works

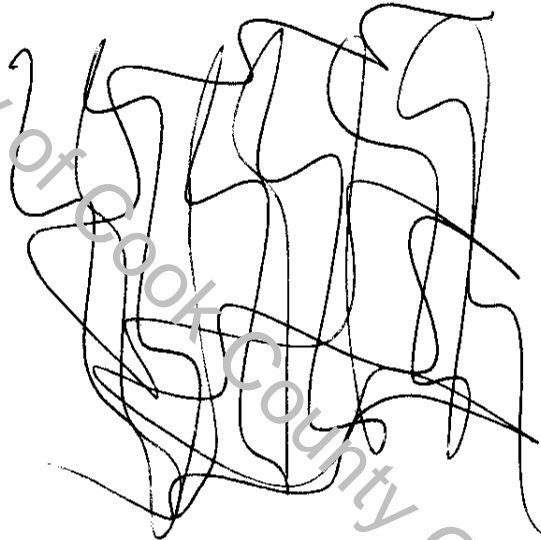
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EXHIBIT C

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CG-1500 (C9)

COMPLETION REPORT

UNIT'S CONTROL SYMBOL
OAR-2143 (C9)

NINTH COAST GUARD DISTRICT
1240-East Ninth Street
Cleveland, Ohio 44199

Date substructure commenced 8/2/88
Date substructure completed 9/27/88
Date superstructure commenced 6/27/89
Date superstructure completed 7/25/90

FOR USE ABOVE MOUTH

MILES ABOVE MOUTH	NAME & LOCATION	OWNER	TYPE OF BRIDGE	CLEARANCE		CLEARANCE GAGES (Yes or No)	PIER PROTECT. (Yes or No)	DATE PLANS APPROVED AND PERMIT NUMBER	TYPE OF TRAFFIC
				HORI. ZONTAL	VERTICAL				
5.77	Southwest Rapid Transit (CTA)	City of Chicago	F	12'	18'			5/20/87 (3-87-9)	RR 10/92 date opened to traf- fic:

Bridge was removed from the waterway in its entirety. All parts of the old bridge not incorporated in the new structure were removed to a depth of _____ feet below Low Water Datum / to the natural bottom of the waterway. (If any parts of the old bridge are not incorporated in the new structure but retained in the waterway, include a copy of the Corps of Engineers' permit or letter authorizing retention).

Certify that the above bridge has been built in accordance with the approved plans (or in accordance with USCG approved modifications to the original permit) and that all conditions of the permit have been fulfilled. Photographs of the completed structure as seen by the mariner (from upstream and downstream) are herewith submitted.

Additional remarks:
Datum: Mean Tide New York
1935 Adj. at Chicago

Commissioner
Chicago Department of Transportation

SIGNATURE: *[Signature]*
DATE: 8/17/92


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EXHIBIT D

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BRIDGE PERMIT PARCEL

THAT PART OF THE SOUTHWEST 1/4 OF SECTION 29, TOWNSHIP 39 NORTH, RANGE 14 EAST OF THE THIRD PRINCIPAL MERIDIAN EXTENDING ACROSS AND SPANNING THE SOUTH FORK OF THE SOUTH BRANCH OF THE CHICAGO RIVER THAT LIES BETWEEN TRACTS OR PARCELS OF LAND AND AIR RIGHTS PARCELS ABUTTING THE EASTERLY AND WESTERLY BANKS OR DOCK LINES OF SAID SOUTH FORK OF THE SOUTH BRANCH OF THE CHICAGO RIVER, SAID PARCELS BEING ON PART OF THE RIGHTS OF WAY OF THE ILLINOIS CENTRAL GULF RAILROAD COMPANY AND ATCHISON, TOPEKA & SANTA FE RAILWAY COMPANY ALONG WITH A PARCEL ADJOINING SAID RIGHTS OF WAY AND AN AIR SPACE PERMIT FROM THE STATE OF ILLINOIS EXTENDING NORTHEASTERLY FROM S. ASHLAND AVENUE (NORTH OF ARCHER AVENUE) TO THE EAST LINE OF SAID SOUTHWEST 1/4 ACROSS SAID SOUTHWEST 1/4 AS DESCRIBED IN THE DOCUMENTS LISTED BELOW:

PARCEL #	TYPE PARCEL	TYPE OF DOC.	DOC. #	REC. DATE
IC804A	Fee Taking	Final Judge Order	330431063	10/31/2003
SF301A	Fee Taking	Final Judge Order	331118061	11/7/2003
	Fee Taking	Quitclaim Deed	93328879	5/4/1993
	Fee Taking	Quitclaim Deed	93328880	5/4/1993
	Fee Taking	Quitclaim Deed	93328881	5/4/1993
PU101D	Air Space	Air Space Permit		3/6/1990

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